



THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND	
STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES	2C
SPECIAL NOTES	2D, 2D1, 2D2
ENVIRONMENTAL NOTES	2E, 2E1
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1

YEAR	PROJECT NO.	SHEET NO.
2025	NH-I-40-4(93)	ROADWAY-SIGN 1
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET		

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DAVIDSON COUNTY

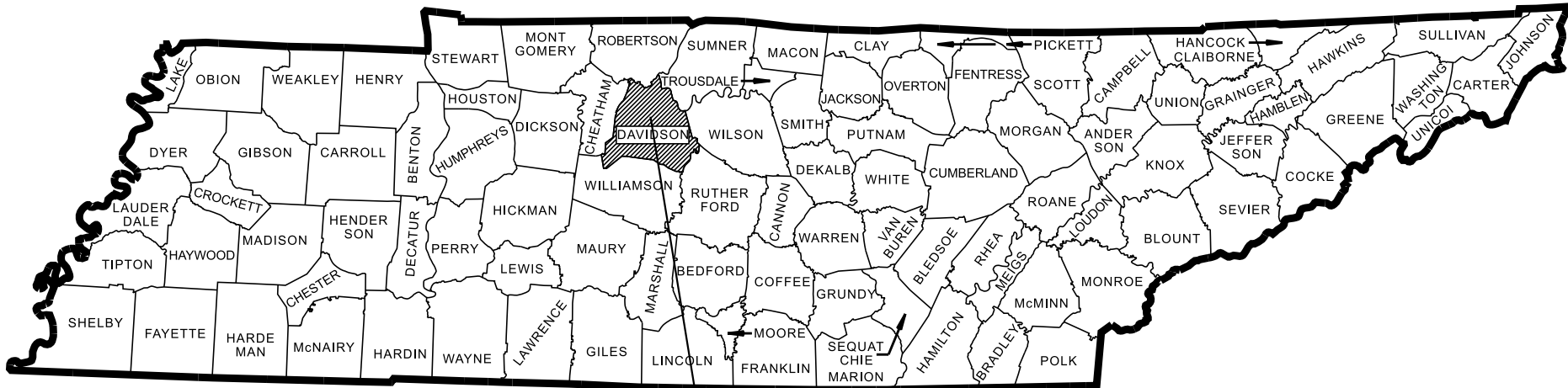
INTERSTATE 40
FROM NEAR JEFFERSON STREET UNDERPASS
TO BRIDGE OVER SR-6 (8TH AVENUE SOUTH)

RESURFACE
MILL, BM2, 411D, AND PAVEMENT MARKING

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-40

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH-I-40-4(93)	
STATE PROJ. NO.	19I040-F8-024	



PROJECT LOCATION

19I040-F8-024
BEGIN PROJECT NO. NH-I-40-4(93) RESURFACE
I-40 (L.M. 16.44)

NASHVILLE & EASTERN RAILROAD (NERR) CROSSING #298481V
I-40 L.M. 16.62
NERR (N 0000.640)
LAT: 36.166943 LONG: -86.795613

CSX TRANSPORTATION (CSXT) CROSSING #348013R
I-40 L.M. 16.75
CSXT (00N 0000.530)
LAT: 36.165686 LONG: -86.794739

19I040-F8-024
END PROJECT NO. NH-I-40-4(93) RESURFACE
I-40 (L.M. 18.40)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER: KONNER SPRADLIN, P.E., REGION 3

DESIGN FIRM : HDR ENGINEERING, INC.

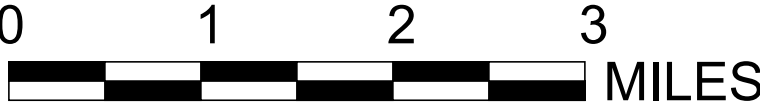
DESIGNER : DANIEL K. MCGAHA, P.E. CHECKED BY DAVID HORNE, P.E.

P.E. NO. 98034-4194-04

PIN NO. 135004.00



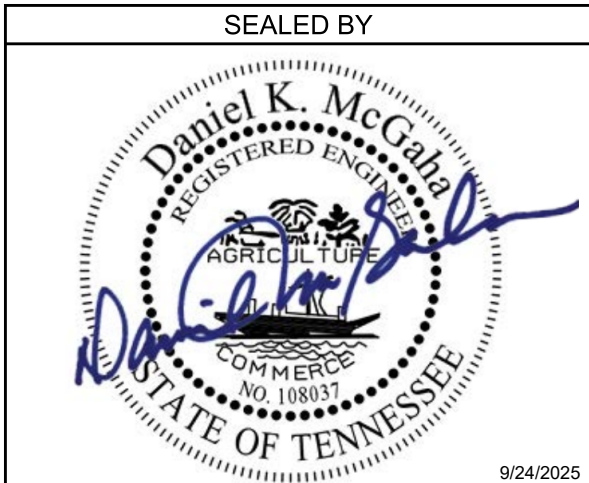
SCALE: 1"= 1 MILE



PROJECT LENGTH 1.96 MILES
TOTAL LANE MILES RESURFACED 16.02 MILES

NO EXCLUSIONS

BRIDGE ID. #	19I00400061	19I00400335	19I00400065
	19I00400067	19I00400069	19I00400328
	19I00400327	19I00400071	19I00400073
	19I00400075	19I00400077	19I00400079
	19I00400080	19I00400317	19I00400081
	19I00400337		



APPROVED:

Shane Hester

SHANE HESTER, CHIEF ENGINEER

DATE:

APPROVED:

Will Reid

WILL REID, COMMISSIONER

TRAFFIC COUNTER, EMBEDDED DETECTION LOOPS & AUTOMATIC TRAFFIC READERS	
STATION LOCATION	LOG MILE
TC STATION 317	16.751
TC STATION 330	17.359
ATR #483 WB	18.235
ATR #283 EB	18.240

TRAFFIC DATA	
ADT (2025)	151318
POSTED SPEED	55 MPH

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D, 2D1, 2D2
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES	2F
PAVEMENT MARKING IMPROVEMENTS.....	2G, 2G1-2G7
TRAFFIC DETECTION LOOPS DETAIL.....	2G8
UTILITY NOTES, AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1
NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.	
NO UTILITY SHEETS ARE INCLUDED IN THIS SET OF PLANS.	
NO PROJECT COMMITMENT SHEETS ARE INCLUDED IN THIS SET OF PLANS.	

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
10-103.00 CATCH BASINS AND MANHOLES		
D-CBB-31	02-20-20	TYPE “B” CAST IRON FRAME, GRATE & INLET DETAILS FOR NOS. 31, 41, 45, 46, & 51 TYPE CATCH BASINS

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
10-201.00 SIGNALS		
T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	07-11-17	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
10-204.00 DESIGN - TRAFFIC CONTROL		
T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-M-6	01-24-25	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-7	01-24-25	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	01-24-25	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-15	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-13	03-26-25	TWO-OUTSIDE LANE CLOSURE FOR EXPRESSWAY AND FREEWAYS
T-WZ-63	03-26-25	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64	03-26-25	WORK ZONE IN THE VICINITY OF AN EXIT
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	1A

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9/24/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,
STANDARD ROADWAY
DRAWINGS AND
STANDARD TRAFFIC
DESIGN DRAWINGS

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
			19I040-F8-024
(1)(2)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	3.1
	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	404
(3)	307-03.08 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON	19711
(4)	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	131
(5)	411-03.10 ACS MIX (PG76-22) GRADING D	TON	11700
(6)	411-01.50 VOID REDUCING ASPHALT MEMBRANE (VRAM) FULL-WIDTH	L.F.	89760
	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	5.6
(7)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	28510
(8)(9)	611-03.04 GRAY IRON CASTINGS (CATCHBASIN)	LB.	2000
(10)	712-01 TRAFFIC CONTROL	LS	1
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	310
	712-05.03 WARNING LIGHTS (TYPE C)	EACH	52
(11)	712-06 SIGNS (CONSTRUCTION)	S.F.	2592
(12)	712-06.16 SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	6
	712-07.03 TEMPORARY BARRICADES (TYPE III)	L.F.	48
	712-08.03 ARROW BOARD (TYPE C)	EACH	4
	712-08.09 DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	6
	712-08.12 QUEUE PROTECTION TRUCK	DAY	120
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	4
	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	1130
	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	1130
(13)(14)(15)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	46
(14)(15)(16)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	9
(14)(15)	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	2595
(14)(15)(16)	716-02.11 PLASTIC PAVEMENT MARKING (6" DOTTED LINE)	L.F.	6028
(14)(15)(16)	716-04.01 PLASTIC PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	5
(14)(15)(16)	716-04.14 PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH	2
(17)	716-05.02 PAINTED PAVEMENT MARKING (8" BARRIER LINE)	L.F.	9825
(17)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	20.2
(15)(16)	716-10.50 PREFORMED PLASTIC PAVEMENT MARKING (INTERSTATE SHIELD)	EACH	16
(16)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	10.1
(16)	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	9825
(15)(16)	716-12.09 ENHANCED FLAT LINE THERMO (12IN LINE)	L.F.	1348
(15)(16)	716-12.10 ENHANCED FLAT LINE THERMO (12IN DOTTED)	L.F.	3596
	717-01 MOBILIZATION	LS	1
	730-03.20 INSTALL PULL BOX (TYPE A)	EACH	2
	730-12.02 CONDUIT 2" DIAMETER (PVC SCHEDULE 40)	L.F.	40
	730-14.02 SAW SLOT	L.F.	750
	730-14.03 LOOP WIRE	L.F.	3000

THERE IS NO GUARDRAIL WORK ON THIS PROJECT

FOOTNOTES	
(1)	TO BE USED AS DIRECTED BY THE ENGINEER.
(2)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2C, PAVEMENT MARKINGS, FINAL PAVEMENT MARKING NOTE 6 AND SHEET 2C, PAVEMENT, RESURFACING, NOTE 2.
(3)	INCLUDES 1500 TONS FOR SPOT LEVELING.
(4)	INCLUDES 7 TONS FOR RAMP SHOULDERS.
(5)	INCLUDES 1022 TONS FOR RAMP SHOULDERS.
(6)	THIS WILL BE USED ONLY AT THE LONGTIUDINAL JOINT BETWEEN TRAVEL LANES AND WILL BE PLACED IN 18" WIDE STRIPS ON THE LONGTUDINAL JOINT AS EACH LANE IS PAVED.
(7)	INCLUDES 1013 TONS FOR RAMP SHOULDERS.
(8)	COST TO INCLUDE REMOVAL OF EXISTING GRATE AND REPLACEMENT OF GRATES THAT DO NOT CONFORM TO STANDARD DRAWING D-CBB-31 WHERE APPLICABLE, INCLUDING MATERIAL, PARTS, LABOR, EQUIPMENT, MACHINERY, TOOLS, OR APPARATUS NECESSARY FOR REMOVAL AND INSTALLATION. THE CONTRACTOR SHALL VERIFY GRATE DIMENSIONS IN THE FIELD.
(9)	FOR THE REPLACEMENT OF 8 GRAY IRON CASTINGS.
(10)	THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
(11)	THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
(12)	ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS, AND TWO TYPE "B" FLASHERS PER THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.
(13)	ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
(14)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(15)	ITEM TO BE USED AS PERMANENT MARKING ONLY.
(16)	SEE SERIES 2G, PAVEMENT MARKING IMPROVEMENTS, FOR LOCATIONS.
(17)	ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2

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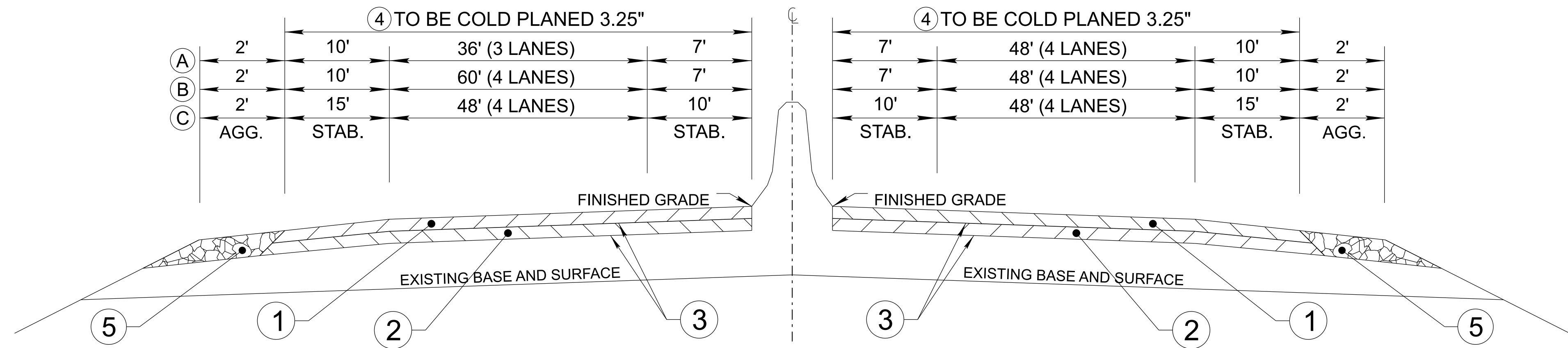


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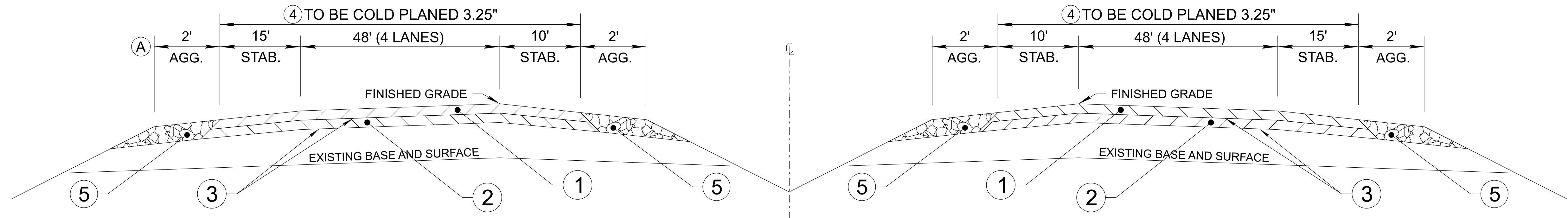
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

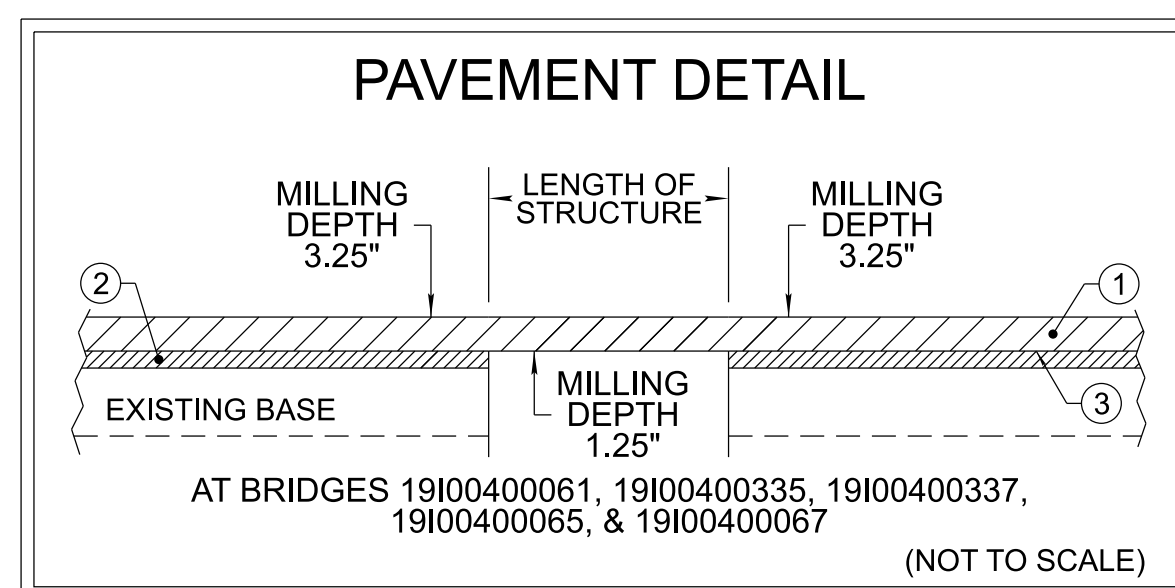
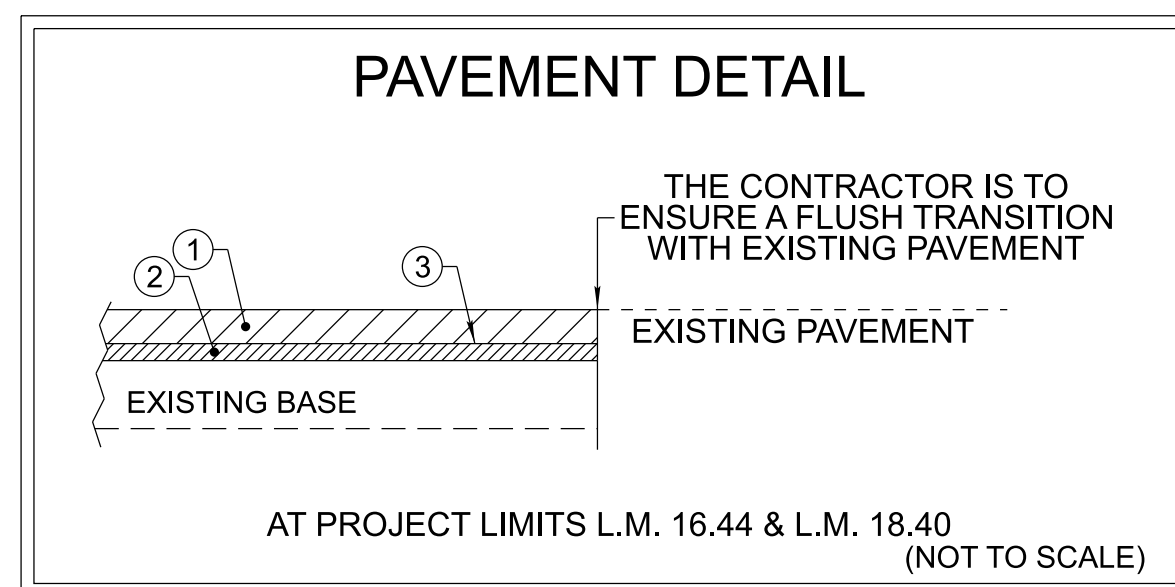
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2B



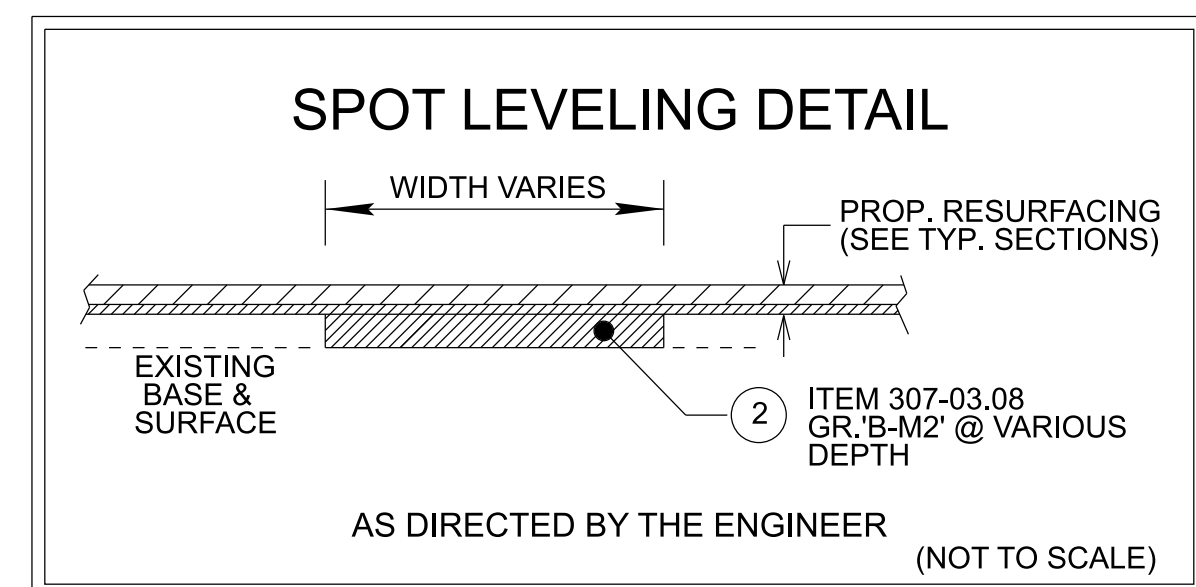
☐ (A) FROM: L.M. 16.44 TO L.M. 16.80
☐ (B) FROM: L.M. 16.80 TO L.M. 17.15
☐ (C) FROM: L.M. 17.15 TO L.M. 17.95



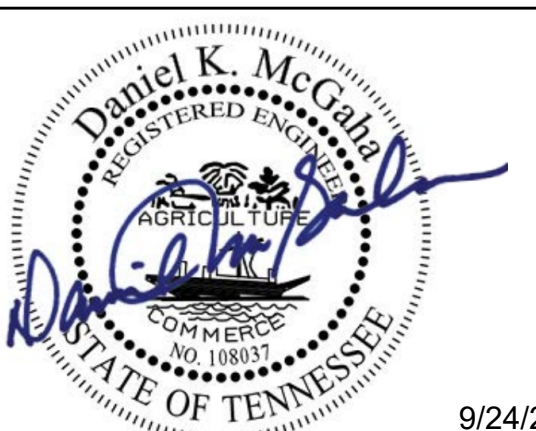
Ⓐ FROM: L.M. 17.95 TO L.M. 18.40



PROPOSED PAVEMENT SCHEDULE	
①	ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D"
②	BITUMINOUS COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.0 LBS./S.Y.) ITEM 307-03.08 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING "B-M2"
③	TACK COAT ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
④	COLD PLANING @ 3.25"± THICK (APPROX. 341.25 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
⑤	MINERAL AGGREGATE @ 2.00"± THICK FOR SHOULDERS ITEM 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"



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24/2025

NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS AND PAVEMENT SCHEDULE

GENERAL NOTES

GRADING

- (1)

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3)

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOODWAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (3)

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2)

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.
- (3)

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.02, PAINTED PAVEMENT MARKING (8” BARRIER LINE), L.F.

FINAL PAVEMENT MARKING

- (6)

THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a.

BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE ENGINEER.

b.

ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE ENGINEER.

c.

ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d.

REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8)

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

- (9)

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19)

REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2)

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (9)

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12)

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2)

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3)

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4)

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5)

USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.

- (6)

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADTS LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7)

ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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GENERAL
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SPECIAL NOTES

PAVEMENT

RESURFACING

- (1)

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:

A.

THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.

B.

THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.

C.

THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.

D.

RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.

E.

ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS

F.

IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

G.

ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (2)

THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

A.

BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.

B.

ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

C.

ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
- (3)

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (7)

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.
- (10)

FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.
- (11)

TEMPORARY RAMP CLOSURES FOR MAINLINE RESURFACING SHALL BE AS FOLLOW:

A.

RAMP CLOSURES SHOULD BE LIMITED TO NO MORE THAN 3 HOURS.

B.

ALL RAMP CLOSURE REQUESTS SHALL BE MADE AT LEAST 7 WORKING DAYS PRIOR TO THE ANTICIPATED DATE OF CLOSURE.

C.

RAMPS IN THE OPPOSITE DIRECTION OF THE CLOSURES CANNOT BE CLOSED AT SAME TIME AND NO SUBSEQUENT EXIT RAMPS SHALL BE CLOSED UNLESS IT IS LESS THAN 1 MILE FROM RAMP TO RAMP.

D.

RAMPS WITH HOSPITALS OR OTHER EMERGENCY/FIRE/POLICE FACILITIES SHALL BE PASSABLE FOR EMERGENCY VEHICLES WHEN CLOSED TO REGULAR VEHICLE TRAFFIC.

STORM DRAIN STRUCTURES

- (1)

TAPER AROUND ALL CATCH BASINS AS DIRECTED BY THE ENGINEER.

PAVEMENT MARKING

- (1)

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.

SIGNS

- (1)

IF THE CONTRACTOR ELECTS TO UTILIZE SIGNPOST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1)

ITEM 303-01 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.
- (2)

THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.
- (3)

MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGES AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGES SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

JOINT SEALANTS

- (1)

THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (3)

PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (4)

TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

CSXT SAFETY NOTES

- (1)

COMPLY WITH THE CONSTRUCTION SUBMISSION CRITERIA OF THE CSXT PUBLIC INFORMATION DOCUMENT AND CONSTRUCTION REQUIREMENTS OF THE PUBLIC PROJECTS MANUAL, WHICH IS AVAILABLE AT THE FOLLOWING URL: <https://www.csx.com/index.cfm/about-us/property/>
- (2)

ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE DONE ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (3)

ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (4)

CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY.

- (5)

ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (6)

ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (7)

AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.
- (8)

THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPOMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.
- (9)

THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT'S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS.
- (10)

PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD'S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (11)

ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.
- (12)

NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO SET UP TO WORK OR PARK WITHIN BOOM DISTANCE PLUS 15 FEET OF THE CENTERLINE OF TRACK WITHOUT SPECIFIC PERMISSION FROM THE RAILROAD. NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO FOUL TRACK, WORK WITHIN THE FOUL ZONE, OR LIFT A LOAD OVER THE TRACK WITHOUT FLAGGING PROTECTION AND PERMISSION FOR TRACK TIME FROM THE RAILROAD.
- (13)

ALL WORKMEN AND MACHINE OPERATORS SHALL STAY WITH THEIR MACHINES WHEN CRANE OR BOOM EQUIPMENT IS POINTED TOWARD THE TRACK. ALL CRANES AND BOOM EQUIPMENT SHALL STOP WORK AND CLEAR TRACK WHILE TRAIN IS PASSING. SWINGING LOADS SHALL BE SECURED TO PREVENT MOVEMENT WHILE TRAIN IS PASSING AND NO LOADS SHALL BE SUSPENDED ABOVE A MOVING TRAIN. ALL CRANES AND BOOM EQUIPMENT SHALL BE TURNED AWAY FROM THE TRACK AFTER EACH WORK DAY OR WHENEVER UNATTENDED BY AN OPERATOR.
- (14)

ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.
- (15)

"ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE TWO (2) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.

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SPECIAL NOTES

CSXT SAFETY NOTES (CONT.)

- (16) ALL SOILS EXCAVATED WITHIN CSXT’S RAILROAD RIGHT-OF-WAY SHALL REMAIN ON CSXT’S RIGHT-OF-WAY. TESTING OF SOILS ON CSXT ROW IS PROHIBITED WITHOUT PRIOR WRITTEN CSXT AUTHORIZATION. ANY SOILS EXCAVATED ON CSXT ROW CAN BE REUSED ON THE ROW PROVIDED PLACING SOILS ALONG CSXT ROW POSES NO ADVERSE IMPACTS TO THE EXISTING TERRAIN, DRAINAGE OR ENVIRONMENT. SHOULD SOIL NEED TO BE REMOVED FROM CSXT ROW, THE CSXT ENVIRONMENTAL DEPARTMENT WILL SAMPLE THE SOIL FOR DISPOSITION. SOIL STAGED ON CSXT MUST FOLLOW CSXT PROTOCOL AND BE PROPERLY STORED AND/OR PROTECTED FROM THE ELEMENTS AND POTENTIAL EXPOSURE.

THE CONTRACTOR SHALL NOTIFY AND COORDINATE THEIR WORK WITH THE FOLLOWING CSXT REPRESENTATIVE:

CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE
CROUCH ENGINEERING, INC.
5115 MARYLAND WAY, SUITE 225
BRENTWOOD, TN 37027
ATTN: MR. SCOTT VICK, P.E. PROJECT DIRECTOR
PHONE: (615) 791-0630
EMAIL: SVICK@CROUCHENGINEERING.COM

NASHVILLE & EASTERN COMPANY RAILROAD CROSSING NOTES

- (1) ANY PAVEMENT PREPARATION, PAVEMENT INSTALLATION, PAVEMENT MARKINGS AND/OR RAILROAD WARNING SIGN INSTALLATION WITHIN 25- FEET OF THE NEAREST NERR TRACK UTILIZING CONTRACTOR EQUIPMENT OR PERSONNEL WILL REQUIRE FULL TIME RAILROAD FLAGGING SERVICES OR AS OTHERWISE DETERMINED BY THE RAILROAD ONLY.

- (2) SEE AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C FOR WORK UP TO AND NEAR THE NERR CROSSING REGARDING FLAGGING RULES AND INSURANCE REQUIREMENTS.

- (3) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT NERR’S TRACK FACILITIES AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY NERR RAILROAD SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER. THE CONTRACTOR SHALL NOT STORE ANY OF HIS CONSTRUCTION EQUIPMENT ON THE RAILROAD’S RIGHT-OF- WAY. THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVE OF THE RAILROAD:

MR. JIMMY HORTON
TRACK SUPERVISOR
R.J. CORMAN RAILROAD COMPANY/NASHVILLE & EASTERN
PO BOX 788, 101 RJ CORMAN DRIVE
PHONE: 615-445-5357
E-MAIL: JIMMY.HORTON@RJCORMAN.COM

SPECIAL TRAFFIC COUNTER SPECIFICATIONS

- (1) CONTRACTOR SHALL SUPPLY AND INSTALL ALL RELATED EQUIPMENT SO THAT ONCE A TRAFFIC COUNTER AND MODEM ARE INSTALLED BY THE STATE’S PROJECT PLANNING DIVISION’S TRAFFIC COUNTER PERSONNEL, THE COUNT STATION WILL BE FULLY OPERATIONAL.

- (2) COUNTER CABINET SHALL NOT BE PRE-WIRED FOR A TRAFFIC SIGNAL AND SHALL INCLUDE VENTILATION THROUGH THE TOP AND DOOR BUT DOES NOT TO INCLUDE A VENTILATION FAN.

- (3) MINIMUM SIZE OF CABINET SHALL BE 41" TALL, 25" WIDE AND 18" DEEP AND INCLUDE ONE ADJUSTALBE SIDE SHELF. COMPLETE INSTALLATION PER SPECIAL TRAFFIC COUNTER SPECIFICATION NOTES CONCERNING THE COUNTER CABINET. ITEM INCLUDES THE 3" GALVANIZED, RIGID CONDUIT POLE THAT THE CABINET IS TO BE MOUNTED ONTO.

- (4) THE CABINET DOOR SHALL BE SEALED WITH A CLOSED CELL NEOPREME GASKET BONDED TO THE INSIDE OF THE DOOR WITH AN OIL RESISTENT ADHESIVE THAT WILL BE POSITIONED TO PREVENT ANYTHING FROM PENTRATING THROUGH THE CRACK BETWEEN THE DOOR AND THE CABINET WHEN THE DOOR IS CLOSED. CABINET SHALL INCLUDE A THREE POINT LOCKING SYSTEM AND BE MOUNTED SO THAT THE CABINET DOOR WILL OPEN TOWARD THE ROADWAY.

- (5) THE CABINET SHALL BE POLE-MOUNTED TO A 3" GALVANIZED, RIGID CONDUIT POLE WITH THE BOTTOM OF THE CABINET AT LEAST 28" ABOVE GROUND LEVEL. TOP OF THIS POLE SHALL BE AT LEAST EIGHT (14') FEET ABOVE THE GROUND. A 65 WATT SOLAR PANELSHALL BE MOUNTED ON THE 3” RIGID CONDUIT, ALL SOLAR PANEL WIRING SHALL BE RUN THROUGH A WEATHERHEAD AND CONDUIT DOWN THE POLE TO INSIDE THE CABINET AND TERMINATED TO A VOLTAGE REGULATOR. A 12 VOLT 70-90 AMP HOUR DEEP DISCHARGE MARINE BATTERY SHALL BE INCLUDED.

- (6) ALL LOOPS SHALL BE FIVE FEET BY SEVEN FEET (5' X 7') AND TYPICAL IN SIZE WITH THREE (3) TURNS OF LOOP WIRE.

- (7) EACH LOOP DETECTOR LEAD-IN (OR SHIELDED DETECTOR CABLE) SHALL BE MARKED WITH LANE IDENTIFICATION, DENOTING THE LANE AND POSITION IN THE LANE FOR EACH LOOP (SUCH AS EB-1 FOR EASTBOUND 1, ECT.).

- (8) FOR QUESTIONS CONCERNING ANY OF THE INSTALLATION OF THE COUNTER EQUIPMENT, PLEASE CONTACT STANLEY DUNN AT 615-350-4571.

- (9) THE CONTRACTOR SHALL NOTIFY TO REPORT WHEN WORK HAS BEEN COMPLETED SO LONG RANGE PALANNING CAN FINISH THE INSTALLATION.

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VOID REDUCING ASPHALT MEMBRANE SPECIAL NOTES

I. DESCRIPTION:

- (1) THIS SPECIFICATION COVERS THE REQUIREMENTS AND PRACTICES FOR APPLYING A VOID REDUCING ASPHALT MEMBRANE (VRAM) UNDERNEATH THE LONGITUDINAL CONSTRUCTION JOINT OF A COURSE OF A HOT-MIX ASPHALT (HMA) PAVEMENT. THE VRAM SHALL BE APPLIED BENEATH THE INTENDED AREA OF THE HMA PAVEMENT LONGITUDINAL CONSTRUCTION JOINT PRIOR TO THE INSTALLATION OF THE HMA COURSE. THIS WORK SHALL BE DONE ACCORDING TO THE STANDARD SPECIFICATIONS EXCEPT AS MODIFIED HEREIN.

II. MATERIALS:

- (1) THE BITUMINOUS MATERIAL USED FOR THE VRAM SHALL MEET THE REQUIREMENTS OF THE FOLLOWING TABLE. ELASTOMERS SHALL BE ADDED TO A BASE ASPHALT AND SHALL BE EITHER A STYRENE-BUTADIENE (SB), STYRENE-BUTADIENE-STYRENE (SBS), OR STYRENE-BUTADIENE RUBBER (SBR) COPOLYMER. IN ADDITION TO THE FOLLOWING TABLE, THE VRAM SHALL BE SUITABLE FOR CONSTRUCTION TRAFFIC TO DRIVE ON WITHIN 30 MINUTES OF PLACEMENT WITHOUT PICKUP OR TRACKING.

TEST	TEST REQUIREMENT	TEST METHOD
DYNAMIC SHEAR @ 88°C (UNAGED), G*/SIN Δ, KPA	1.00 MIN.	AASHTO T 315
CREEP STIFFNESS @ -18°C (UNAGED) STIFFNESS (S), MPA M-VALUE	300 MAX. 0.300 MIN.	AASHTO T 313
ASH, %	1.0 - 4.0	AASHTO T 111
ELASTIC RECOVERY, 100 MM ELONGATION, CUT TRIMMED., 25°C, %	70 MIN.	ASTM D6084 METHOD A
SEPARATION OF POLYMER, DIFFERENCE IN RING AND BALL, °C	3 MAX.	ASTM D7173

III. EQUIPMENT:

- (1) A PRESSURE DISTRIBUTOR SHALL BE PROVIDED THAT IS CAPABLE OF APPLYING THE VRAM AT THE DESIRED APPLICATION RATE. THE DISTRIBUTOR SHALL BE EQUIPPED WITH A HEATING AND RECIRCULATING SYSTEM ALONG WITH A FUNCTIONING AUGER AGITATING SYSTEM OR VERTICAL SHAFT MIXER IN THE HAULING TANK TO PREVENT LOCALIZED OVERHEATING.
- (2) THE CONTRACTOR MAY USE A MELTER KETTLE FOR TRANSPORTING AND/OR APPLICATION OF THE MATERIAL THAT IS CAPABLE OF APPLYING THE VRAM AT THE DESIRED APPLICATION RATE. THE MELTER KETTLE SHALL BE OF AN OIL JACKETED DOUBLE-BOILER TYPE WITH AGITATING AND RECIRCULATING SYSTEMS. MATERIAL FROM THE KETTLE MAY BE DISPENSED THROUGH A PRESSURE FEED WAND WITH AN APPLICATOR SHOE OR SPRAY BAR.

IV. CONSTRUCTION:

- (1) PRIOR TO THE APPLICATION OF THE VRAM, ENSURE THE AREA OF THE INTENDED LONGITUDINAL ASPHALT PAVEMENT JOINT IS THOROUGHLY CLEANED AND FREE OF DEBRIS. THE AREA MAY BE CLEANED BY SWEEPER/VACUUM TRUCK, POWER BROOM, AIR COMPRESSOR OR HAND TO THE SATISFACTION OF THE ENGINEER. ENSURE THE EXISTING SURFACE IS DRY AND FREE OF MOISTURE.
- (2) MILLED SURFACES MAY REQUIRE THE USE OF COMPRESSED AIR TO REMOVE DUST AND FINE MATERIALS FROM THE AREA WHERE VRAM WILL BE APPLIED. FINAL CLEANING WILL BE WITHIN 24 HOURS OF THE PLACEMENT OF VRAM AND PERFORMED TO THE ENGINEER'S APPROVAL.
- (3) THE CENTER OF THE VRAM APPLICATION WIDTH SHALL BE WITHIN 2" OF THE PROJECT ESTABLISHED CENTERLINE OR ESTABLISHED LANE EDGE. A STRINGLINE OR PAINT MARK SHALL BE USED AS A GUIDE FOR APPLICATION IN ORDER TO MAINTAIN A UNIFORM EDGE ALIGNMENT; IF ANY OTHER METHOD IS PROPOSED, IT SHALL MEET THE APPROVAL OF THE ENGINEER BEFORE BEING USED. WHEN ONLY ONE-HALF OF THE JOINT IS EXPOSED, SUCH AS A MILL AND INLAY PROJECT, THE APPLICATION SHALL BE APPLIED AT ONE-HALF THE PRESCRIBED WIDTH AND RATE AND SHALL BE ADJACENT TO THE CENTER OF THE JOINT, AND THE VERTICAL FACE OF THE COLD JOINT LEFT IN PLACE SHALL ALSO BE COATED.
- (4) THE VRAM SHALL BE APPLIED TO THE EXISTING SURFACE PRIOR TO OR FOLLOWING ANY OR ALL TACK COAT APPLICATIONS. TACK COAT MAY BE PLACED OVER THE VRAM AT THE ENGINEER'S DISCRETION. SHOULD THE TACK COAT APPLICATION SUPERSEDE PLACEMENT OF THE VRAM, THE TACK COAT SHALL BE FULLY CURED.

- (5) THE APPLICATION RATE OF VRAM SHALL BE DETERMINED FROM THE JOB MIX FORMULA (JMF) FOR THE PAVING PROJECT. FROM THE JMF, DETERMINE THE NOMINAL MAXIMUM AGGREGATE SIZE (NMAS) FOR THE MIXTURE. NMAS IS DEFINED AS ONE SIEVE SIZE LARGER THAN THE FIRST SIEVE TO RETAIN MORE THAN 10%. FROM THE FOLLOWING TABLE, DETERMINE IF THE MIXTURE IS DEFINED AS FINE-GRADED OR COARSE-GRADED. AFTER DETERMINING THE MIXTURE TYPE, THE APPLICATION RATE TABLE MAY BE REFERENCED.

DEFINITION OF FINE AND COARSE-GRADED MIXTURES*

MIXTURE NMAS	COARSE-GRADED	FINE-GRADED
19MM (3/4")	< 35% PASSING 2.36MM (#8)	≥ 35% PASSING 2.36MM (#8)
12.5MM (1/2")	< 40% PASSING 2.36MM (#8)	≥ 40% PASSING 2.36MM (#8)
9.5MM (3/8")	< 45% PASSING 2.36MM (#8)	≥ 45% PASSING 2.36MM (#8)

* HMA PAVEMENT MIX TYPE SELECTION GUIDE

- (6) THE VRAM, MEETING THE REQUIREMENTS SPECIFIED HEREIN, SHALL BE APPLIED TO THE EXISTING SURFACE AT THE WIDTH AND TARGET APPLICATION RATE AS SPECIFIED IN THE FOLLOWING TABLE:

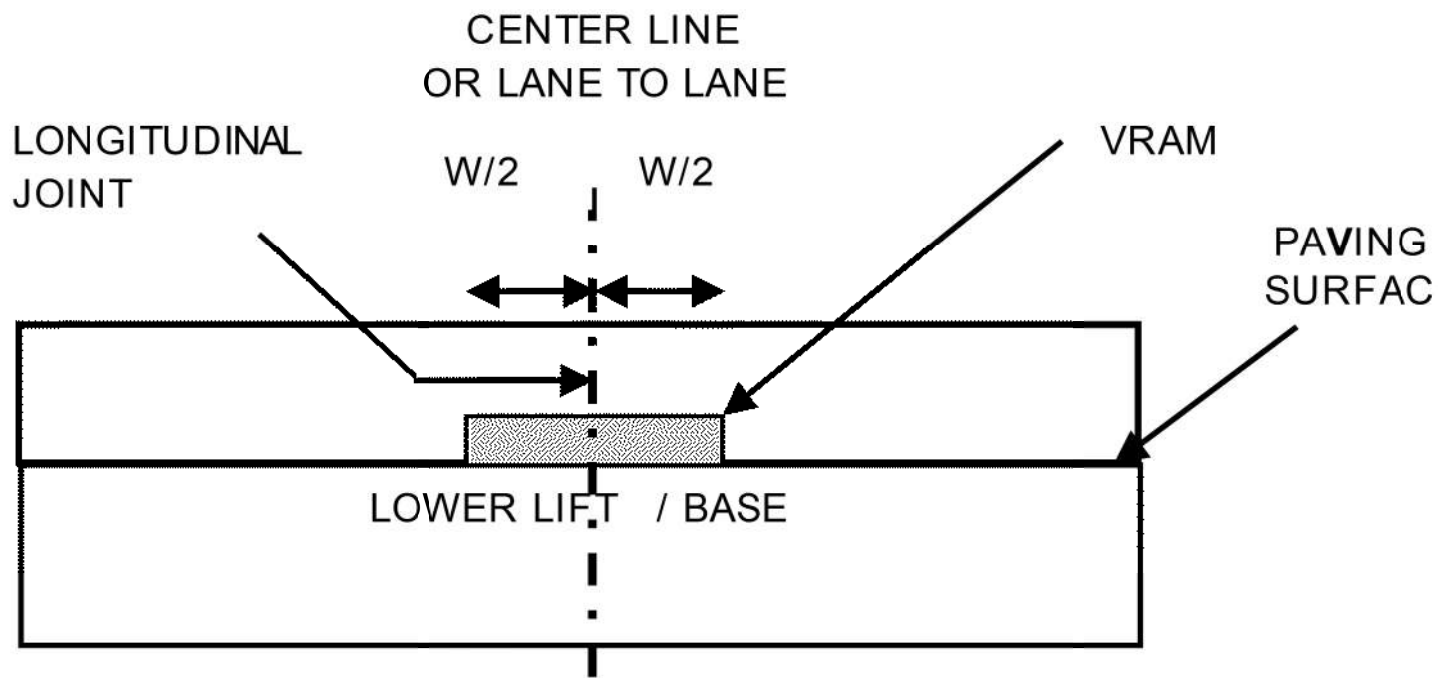
VRAM Application Table ^{1/}		
Coarse-Graded HMA Mixtures ^{2/}		
Overlay Thickness, in	VRAM Width, in.	Application Rate, lb/ft
1	18	1.15
1 ¼	18	1.31
1 ½	18	1.47
1 ¾	18	1.63
≥ 2	18	1.80
Fine-Graded HMA Mixtures ²		
Overlay Thickness, in	VRAM Width, in.	Application Rate, lb/ft
1	18	0.80
1 ¼	18	0.88
≥ 1 ½	18	0.95

NOTES.

A. 1/ EACH APPLICATION RATE HAS A SURFACE DEMAND FOR LIQUID INCLUDED IN IT. THEREFORE, TAKING A GIVEN RATE AND TRYING TO FACTOR IT UP TO A DIFFERENT THICKNESS WILL OVER PREDICT THE REQUIRED AMOUNT OF VRAM.

B. 2/ IN THE EVENT OF A JOINT BETWEEN A COARSE-GRADED MIX AND A FINE-GRADED MIX, USE THE LOWER RATE.

- (7) THE VRAM APPLICATION RATE MAY BE ADJUSTED IN THE FIELD AT THE DISCRETION OF THE ENGINEER BASED ON OBSERVATIONS AND PERFORMANCE.



- (8) THE VRAM SHALL BE APPLIED IN A SINGLE PASS PLACED BY ANY APPLICATION METHOD LISTED IN THE EQUIPMENT SECTION. VRAM APPLICATION TEMPERATURE SHALL NOT EXCEED 330°F.

- (9) THE VRAM SHALL BE APPLIED AT A WIDTH OF NOT LESS OR GREATER THAN 1.5" OF THE WIDTH SPECIFIED IN THE PLANS. IF THE VRAM IS OUTSIDE OF THAT TOLERANCE, STOP AND REMEDIAL ACTION SHALL BE TAKEN SUBJECT TO THE ENGINEER'S APPROVAL.
- (10) THE VRAM SHALL BE SUITABLE FOR CONSTRUCTION TRAFFIC TO DRIVE ON WITHOUT PICK UP OR TRACKING OF THE VRAM WITHIN 30 MINUTES OF PLACEMENT. IF PICK UP OR TRACKING OCCURS, PLACEMENT OF THE VRAM WILL STOP AND REMEDIAL ACTION SHALL BE TAKEN SUBJECT TO THE ENGINEER'S APPROVAL.
- (11) PRIOR TO THE START OF PAVING OF A PAVEMENT COURSE, ENSURE THE PAVER END PLATE AND GRADE CONTROL DEVICE IS ADEQUATELY RAISED ABOVE THE FINISHED HEIGHT OF THE VRAM.
- (12) THE DEPARTMENT WILL REQUIRE A RANDOM SAMPLE OF VRAM. SAMPLES SHALL BE TAKEN AT THE BEGINNING OF THE PROJECT AND EVERY WEEK THEREAFTER. THE SAMPLE SHALL CONSIST OF ONE (1) 1-QUART METAL CONTAINER AND WILL BE TAKEN FROM A RANDOM HAUL TANKER, DISTRIBUTOR OR MELTING KETTLE. SAMPLES SHALL BE TAKEN BY THE CONTRACTOR IN THE PRESENCE OF THE PROJECT INSPECTOR.
- (13) THE CONTRACTOR SHALL SUBMIT EXAMPLES OF PREVIOUS PROJECTS THAT HAVE BEEN SUCCESSFULLY COMPLETED WITH THIS MATERIAL AND APPLICATION EQUIPMENT. IF NO PREVIOUS PROJECTS ARE AVAILABLE FOR REVIEW, THEN A DEMONSTRATION OF THE MATERIAL AND APPLICATION PROCESS SHALL BE SCHEDULED AT THE PRE-CONSTRUCTION CONFERENCE AND COMPLETED PRIOR TO THE START OF THE PROJECT.

V. ACCEPTANCE:

- (1) PROVIDE A BILL OF LADING TO THE ENGINEER FOR EVERY TANKER SUPPLYING MATERIAL TO THE PROJECT.
- (2) THE APPLICATION RATE OF VRAM WILL BE CHECKED BY THE VRAM CONTRACTOR WITHIN THE FIRST 1,000 LINEAR FEET OF THE PROJECT'S APPLICATION LENGTH DURING THE TEST STRIP AND EVERY DAY THEREAFTER. THE RATE WILL BE CHECKED BY WEIGHT PER FOOT. A SUITABLE PAPER OR PAN SHALL BE PLACED AT A RANDOM LOCATION IN THE PATH OF THE PLACEMENT FOR THE VRAM. AFTER APPLICATION OF THE VRAM, THE PAPER OR PAN SHALL BE PICKED UP AND WEIGHED. THE WEIGHT PER FOOT WILL BE CALCULATED. THE TOLERANCE FOR THE PLAN TARGET WEIGHT PER FOOT FROM THE VRAM APPLICATION RATE TABLE SHALL BE ± 10%. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING THE VRAM IN THE AREA WHERE THE APPLICATION RATE WAS CHECKED.

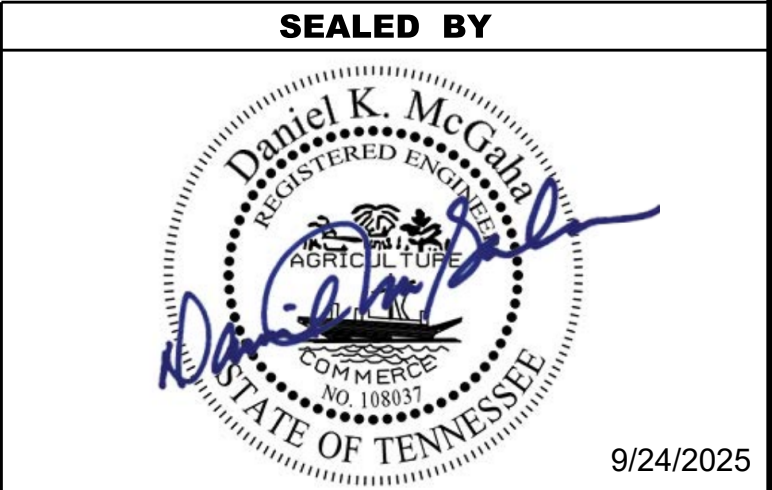
VI. MEASUREMENT:

- (1) THE AGENCY WILL MEASURE THE QUANTITY OF VRAM IN LINEAR FEET.

VII. BASIS OF PAYMENT:

- (1) VOID REDUCING ASPHALT MEMBRANE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT FOR FULL WIDTH APPLICATIONS AS:
- A. 411-01.50 VOID REDUCING ASPHALT MEMBRANE (VRAM) FULL-WIDTH L.F.
- (2) VOID REDUCING ASPHALT MEMBRANE PLACED HALF WIDTH WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT FOR HALF WIDTH APPLICATIONS AS:
- B. 411-01.51 VOID REDUCING ASPHALT MEMBRANE (VRAM) HALF-WIDTH L.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2D2



STATE OF TENNESSEE
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SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4)THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9)THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11)SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15)IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL

- (20)EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2)STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3)STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4)ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (6)THIS PROJECT INCLUDES COLD PLANING, PLACING NEW ASPHALT SURFACE, PAVEMENT MARKINGS, TRAFFIC CONTROL, AND BROOMING AND DEGRASSING.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1)IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6)EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8)THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

- (35)WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44)ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45)FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46)APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47)ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48)THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49)IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50)FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51)IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

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RESURF	2025	NH-I-40-4(93)	2E

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

ENVIRONMENTAL NOTES (CONT.)

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION (CONT.)

- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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RESURF	2025	NH-I-40-4(93)	2E1

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ENVIRONMENTAL
NOTES

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
19I00400061	16.620	HERMAN ST. & MCCORMICK-ASHLAND CITY & NERR	345'-6"	COLD PLANE 1-1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1-1/4" OF NEW ASPHALT.
19I00400335	16.750	CSX RAILROAD	83'	COLD PLANE 1-1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1-1/4" OF NEW ASPHALT.
19I00400337	16.810	CLINTON AVE.	77'	COLD PLANE 1-1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1-1/4" OF NEW ASPHALT.
19I00400065	16.880	JO JOHNSON ST	66'-10-9/16"	COLD PLANE 1-1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1-1/4" OF NEW ASPHALT.
19I00400067	17.090	CHARLOTTE AVE.	132'	COLD PLANE 1-1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1-1/4" OF NEW ASPHALT.
19I00400069	17.330	CHURCH ST. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 15'-6", WBL: 14'-3"
19I00400328	17.400	I-40 RAMP WB LNS. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: WBL: 14'-9"
19I00400328	17.440	I-40 RAMP WB LNS.	226'-0"	LEAVE AS IS (RESTRIPE EXISTING MARKINGS ONLY)
19I00400327	17.450	I-40 RAMP EB LNS. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 15'-4"
19I00400327	17.460	I-40 RAMP EB LNS.	206'-0-3/8"	LEAVE AS IS (RESTRIPE EXISTING MARKINGS ONLY)
19I00400071	17.510	BROAD WAY (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 17'-1", WBL: 14'-6"
19I00400073	17.670	DEMONBREUN (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 15'-2", WBL: 15'-2"
19I00400075	17.870	DIVISION ST. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 15'-7", WBL: 15'-0"
19I00400077	18.010	12TH AVE. SOUTH (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 14'-7", WBL: 14'-7"

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x W			
W20-1	ROAD WORK 1 MILE	48"	x 48"	16	6	96
W20-1	ROAD WORK 1/2 MILE	48"	x 48"	16	6	96
W20-1	ROAD WORK 1000 FT	48"	x 48"	16	6	96
W20-1	ROAD WORK 500 FT	48"	x 48"	16	6	96
W21-2	FRESH OIL - PORTABLE	48"	x 48"	16	6	96
G20-1	ROAD WORK NEXT 2 MILES	48"	x 24"	8	6	48
W8-11	UNEVEN LANES	48"	x 48"	16	20	320
W8-15	GROOVED PAVEMENT	48"	x 48"	16	20	320
W8-15P	MOTORCYCLE PLAQUE	30"	x 24"	5	20	100
W20-1	ROAD WORK AHEAD	48"	x 48"	16	8	128
W3-2	YIELD AHEAD	48"	x 48"	16	6	96
R1-2	YIELD	48"	x 48"	16	6	96
E5-2	EXIT OPEN	48"	x 36"	12	6	72
E5-1	EXIT	72"	x 60"	30	6	180
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x 48"	16	4	64
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	x 48"	16	4	64
W20-5R	RIGHT TWO LANES CLOSED 1/2 MILE	48"	x 48"	16	4	64
W20-5R	RIGHT TWO LANES CLOSED 1500 FT	48"	x 48"	16	4	64
W4-2R	RIGHT LANE ENDS	48"	x 48"	16	4	64
W4-1R	MERGE RIGHT ARROW	48"	x 48"	16	6	96
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x 48"	16	4	64
W20-5L	LEFT LANE CLOSED 1500 FT	48"	x 48"	16	4	64
W20-5L	LEFT TWO LANES CLOSED 1/2 MILE	48"	x 48"	16	4	64
W20-5L	LEFT TWO LANES CLOSED 1500 FT	48"	x 48"	16	4	64
G20-2	END ROAD WORK	48"	x 24"	8	10	80
THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.					TOTAL	2592

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RESURF	2025	NH-I-40-4(93)	2F

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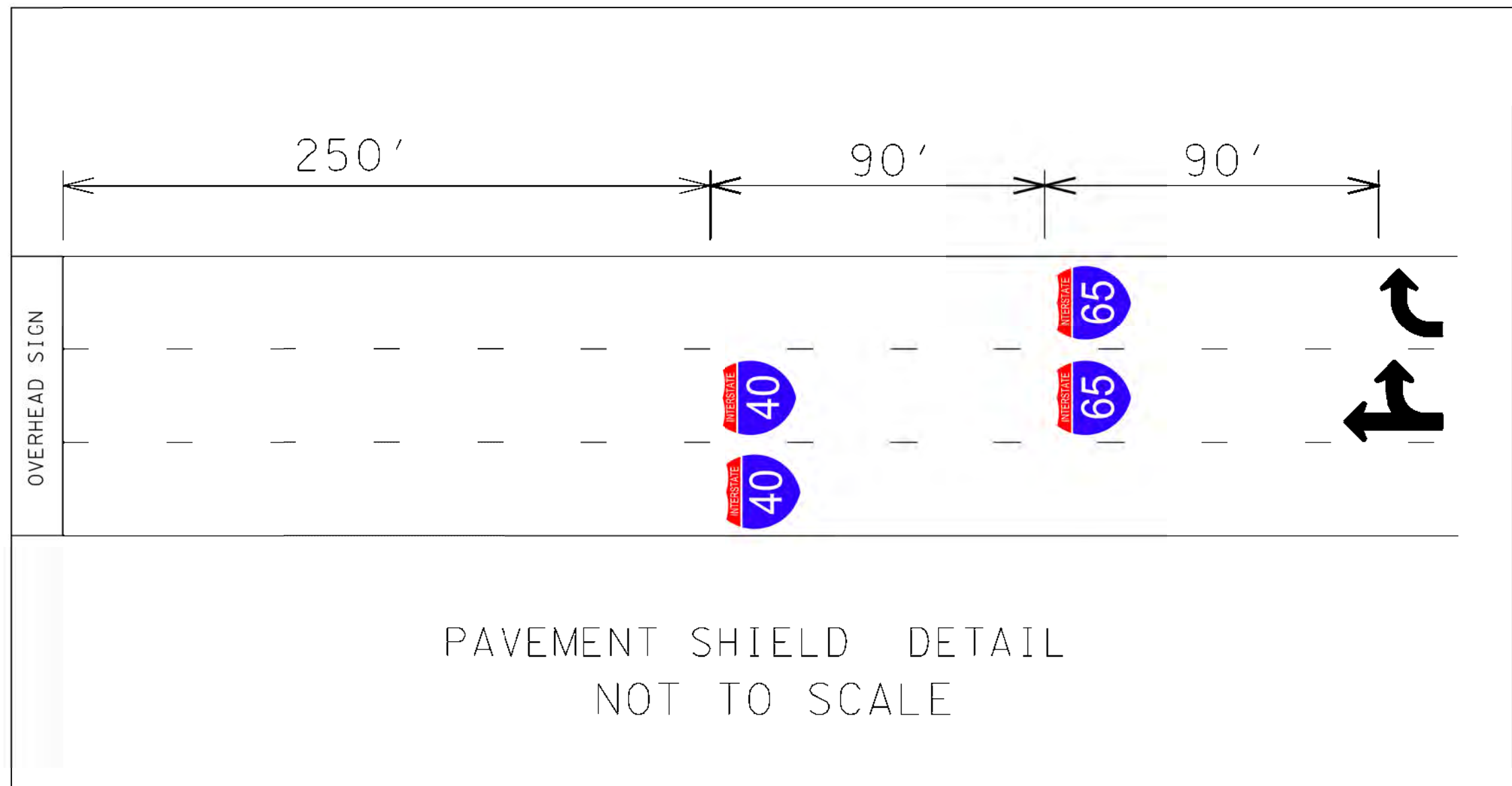
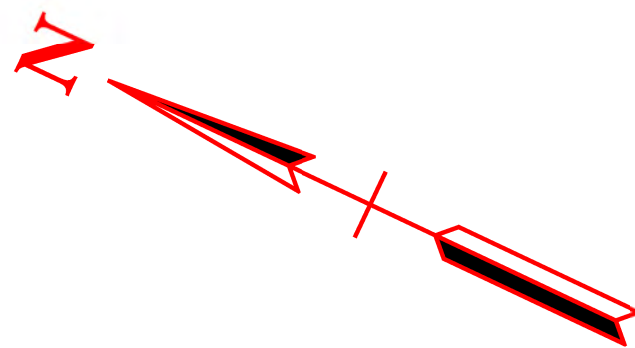
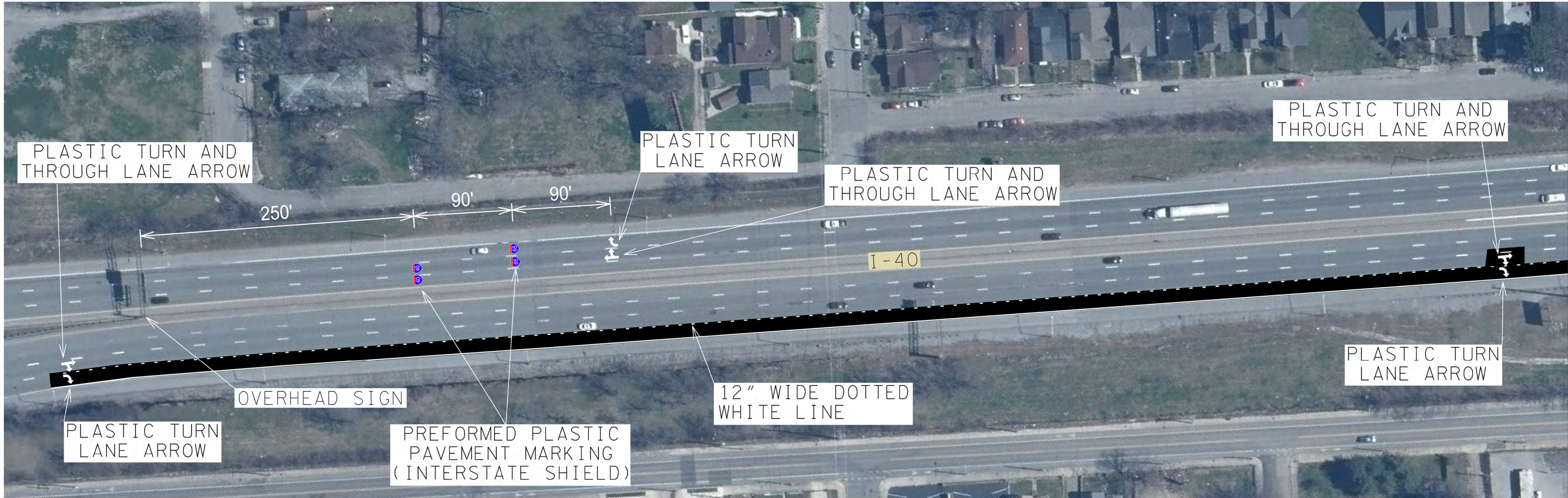


9/24/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G



INTERSTATE 40
16.44 - 16.66 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

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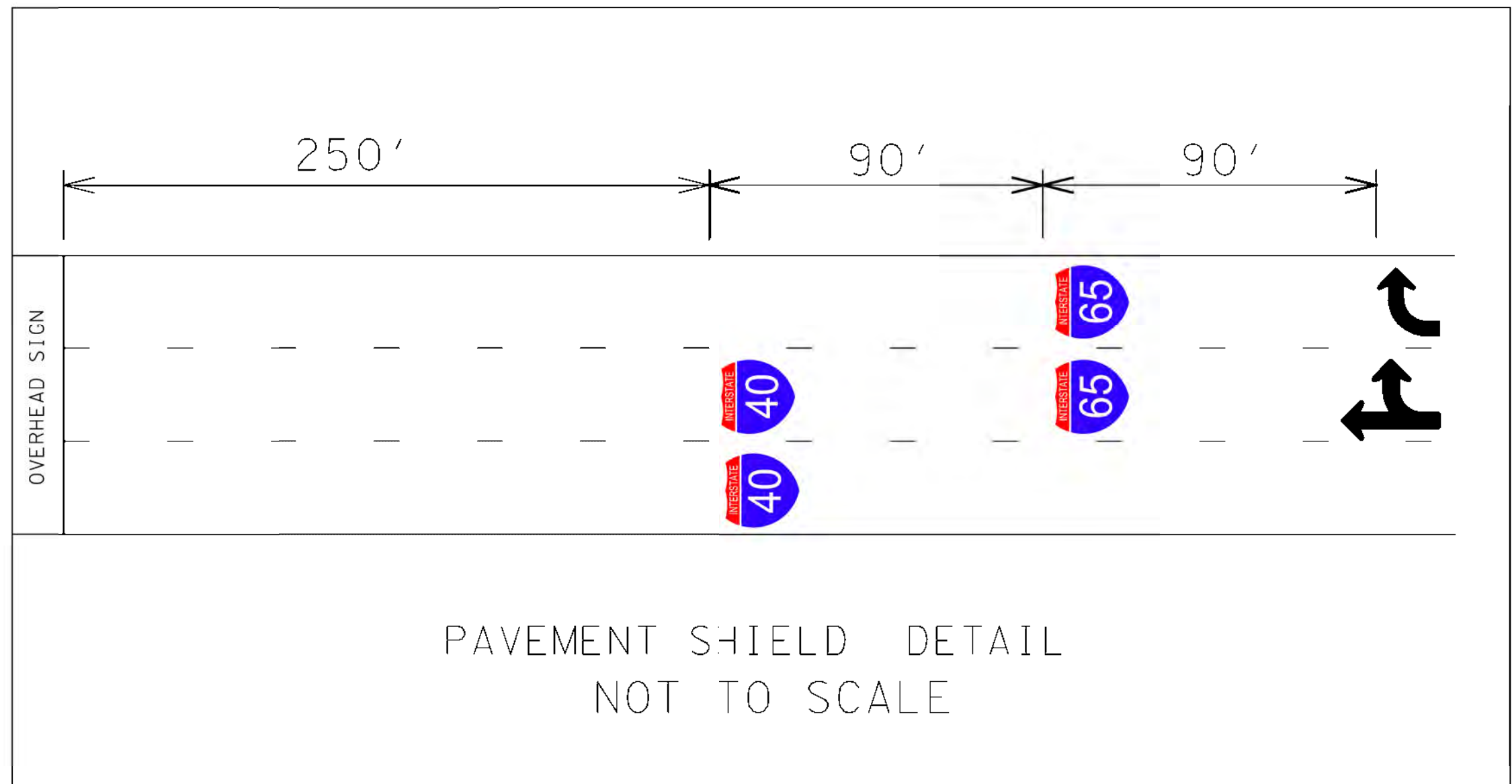
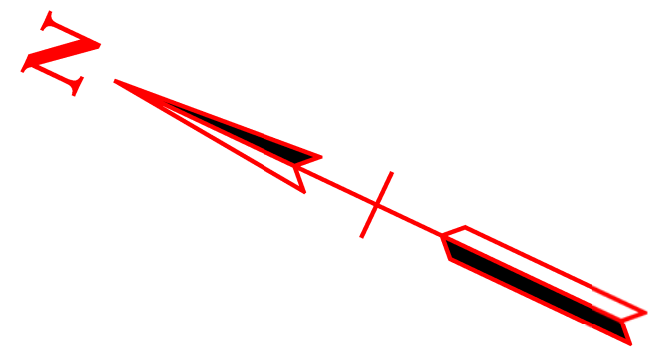
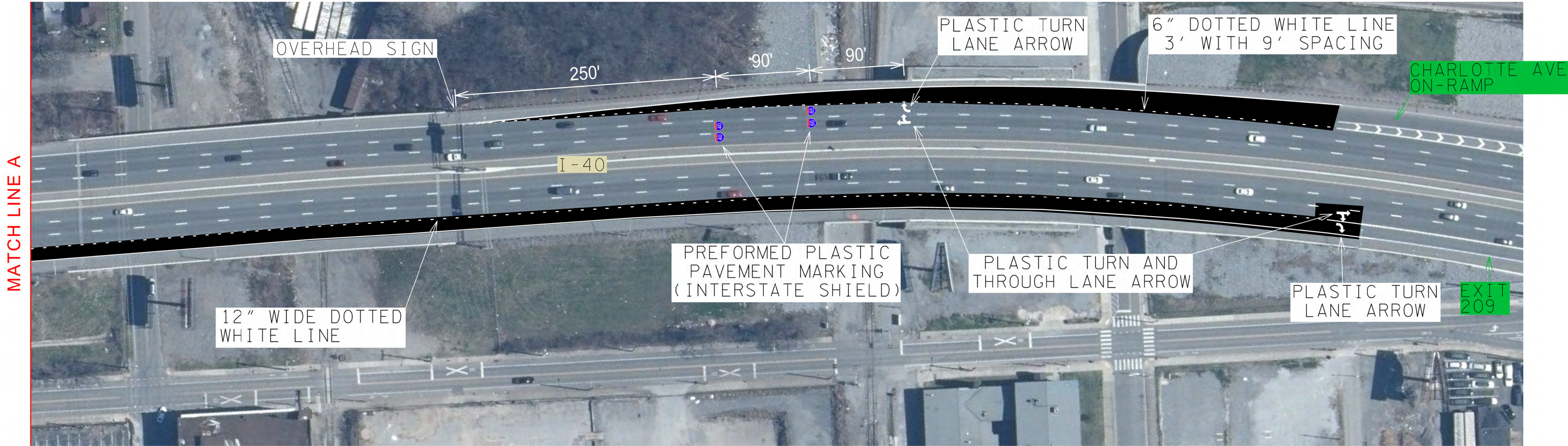
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G1



INTERSTATE 40
16.66 - 16.93 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

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ONLY

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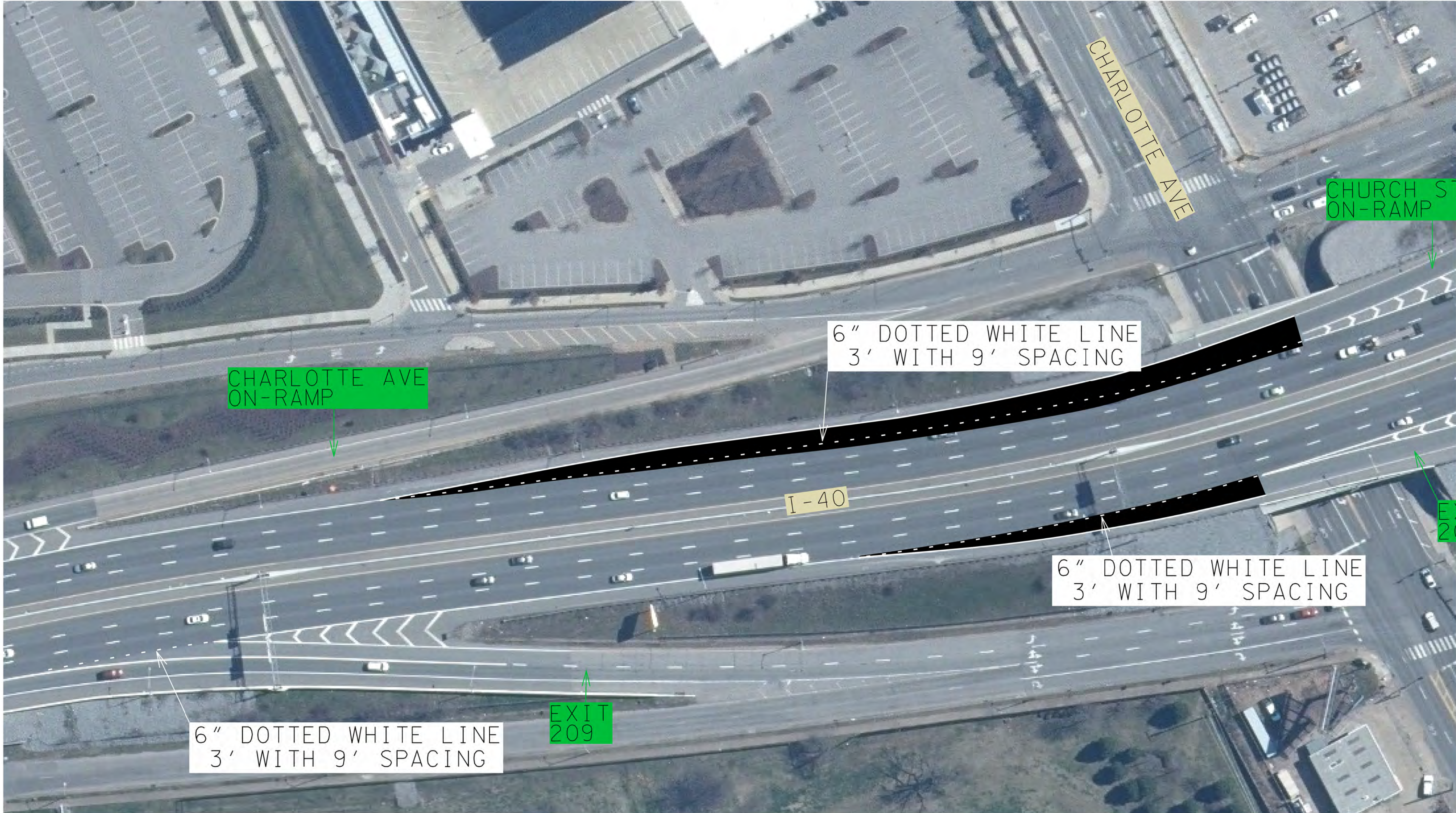
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

9/16/2025 1:34:50 PM
C:\Users\jj10223\Tennessee\Project Development - Documents\Traffic Design\Resurfacing\Projects\2025\DEC\I-40\16.44-18.4\Davidson (135004.00), I-40, 16.44-18.4.dgn

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G2



INTERSTATE 40
16.96 - 17.18 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

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BY TDOT REGION 3 TRAFFIC

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REFERENCE
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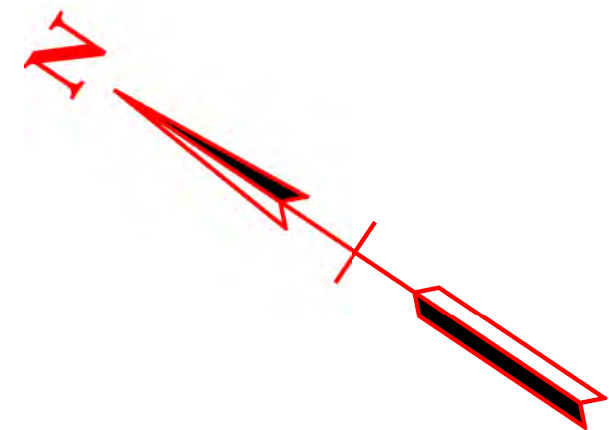
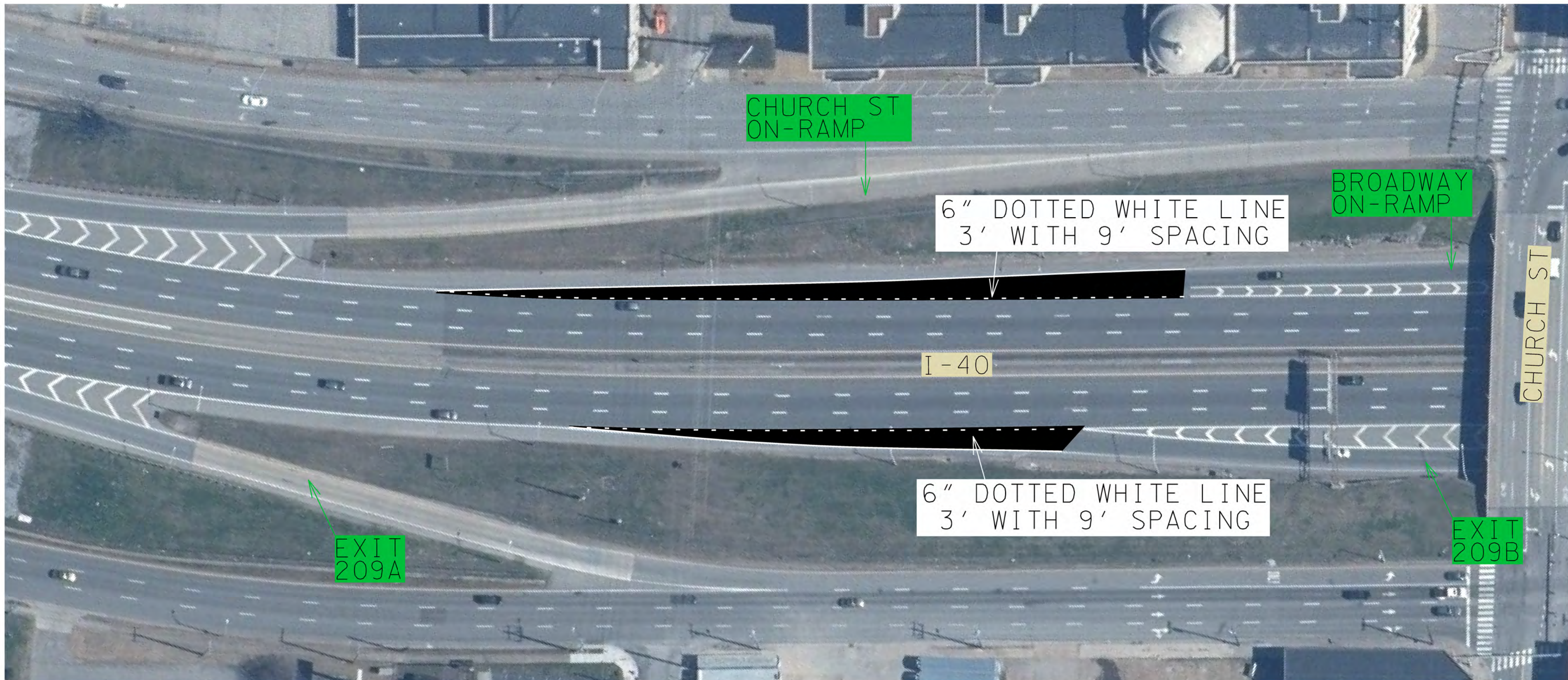
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G3



INTERSTATE 40
17.22 - 17.38 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

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REFERENCE
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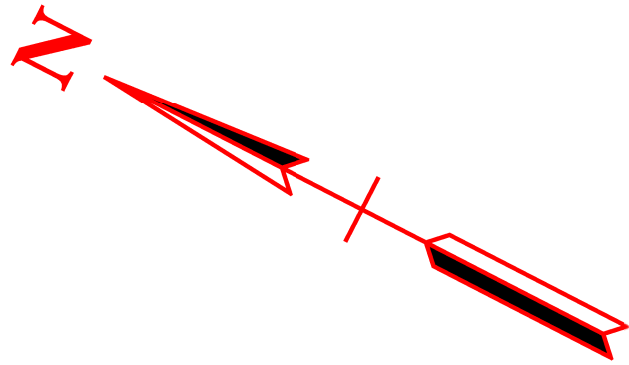
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G4



INTERSTATE 40
17.59 - 17.72 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

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REFERENCE
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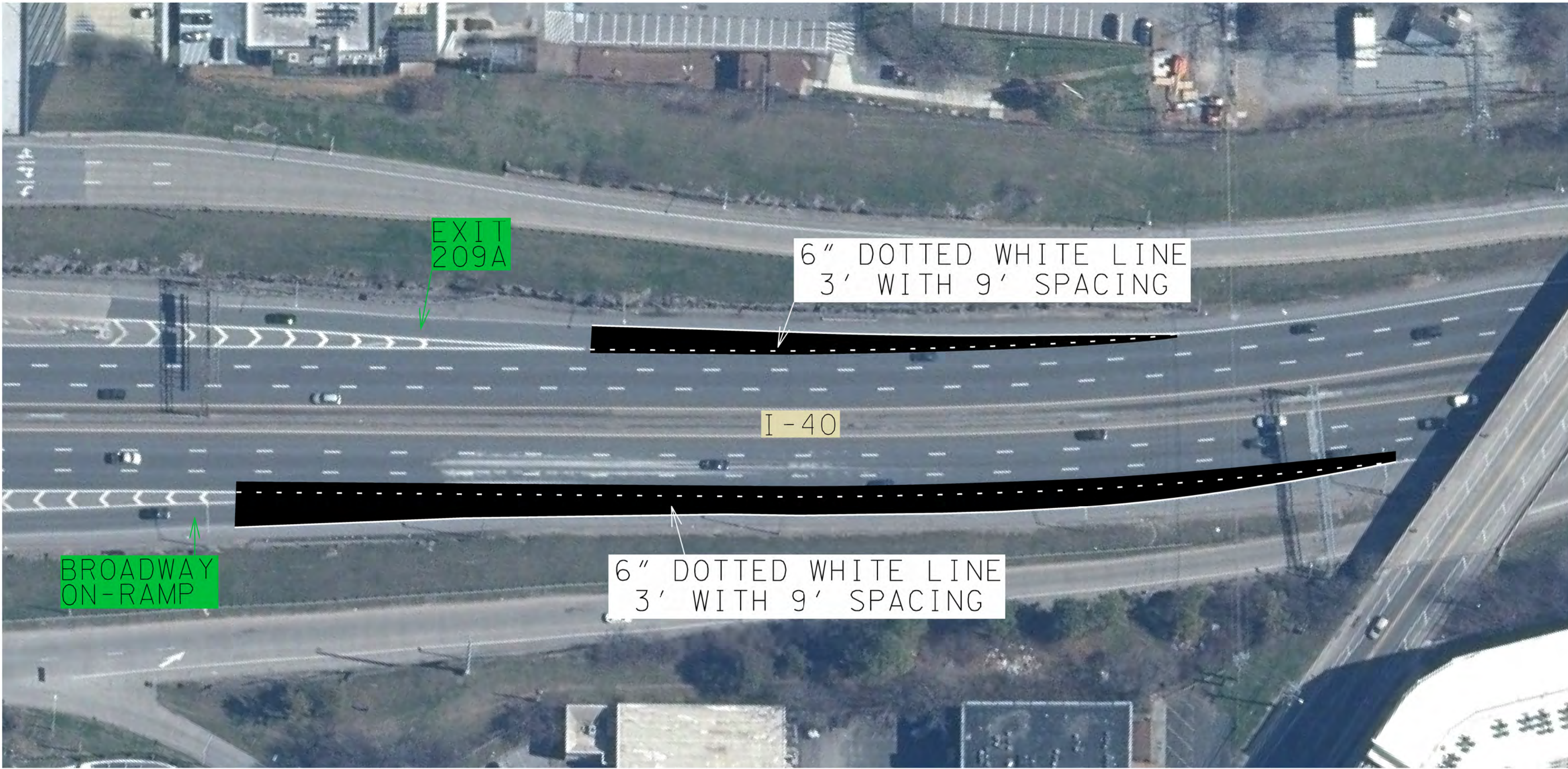
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G5



INTERSTATE 40
17.77 - 17.94 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

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FOR
REFERENCE
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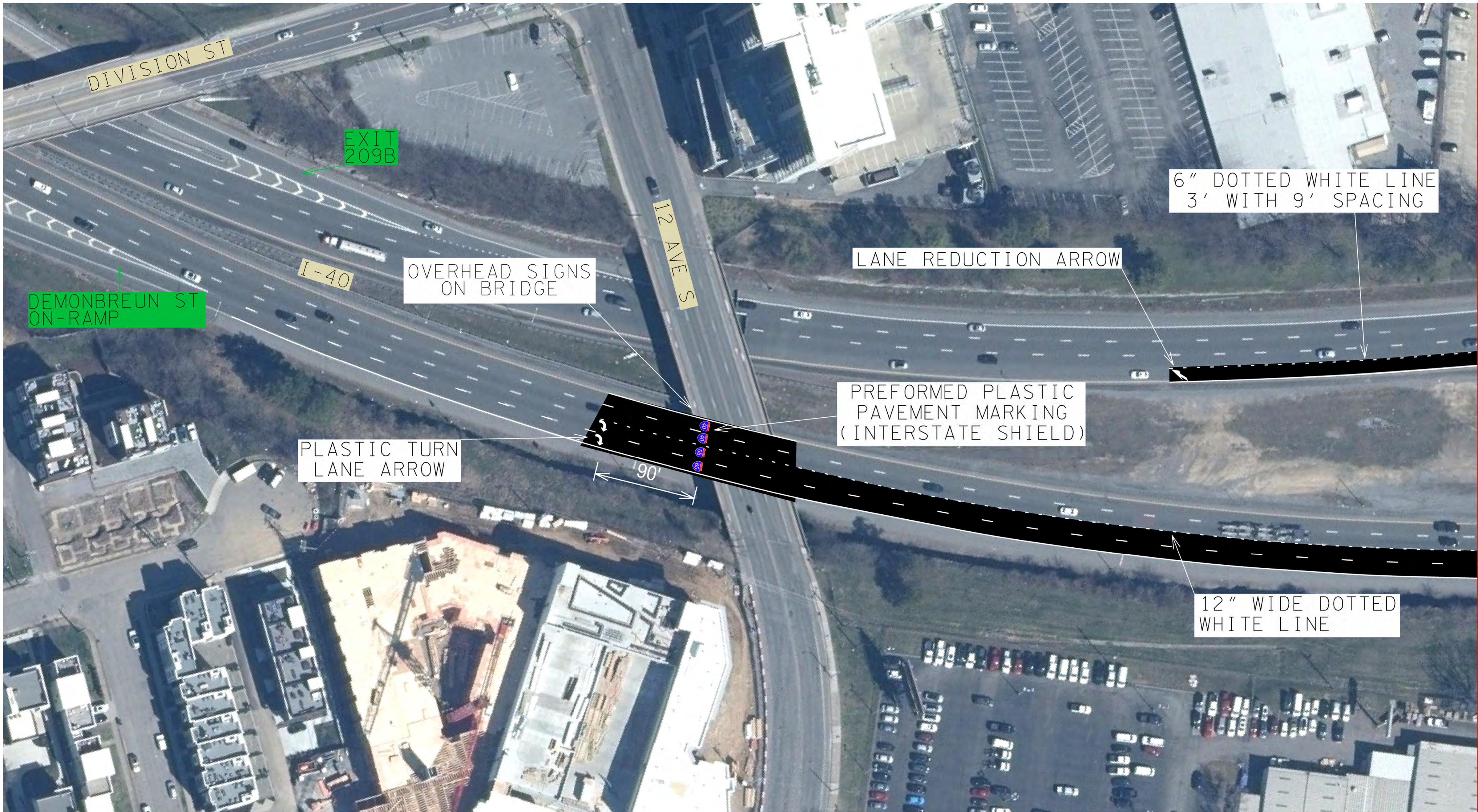
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

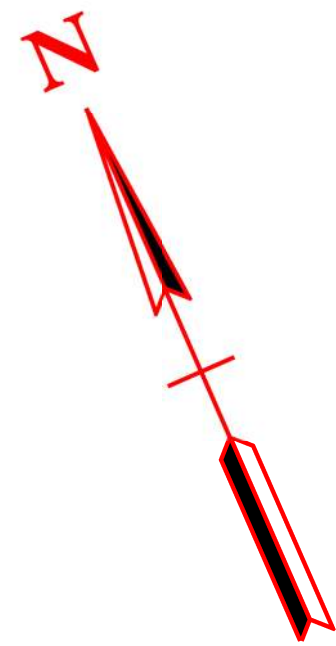
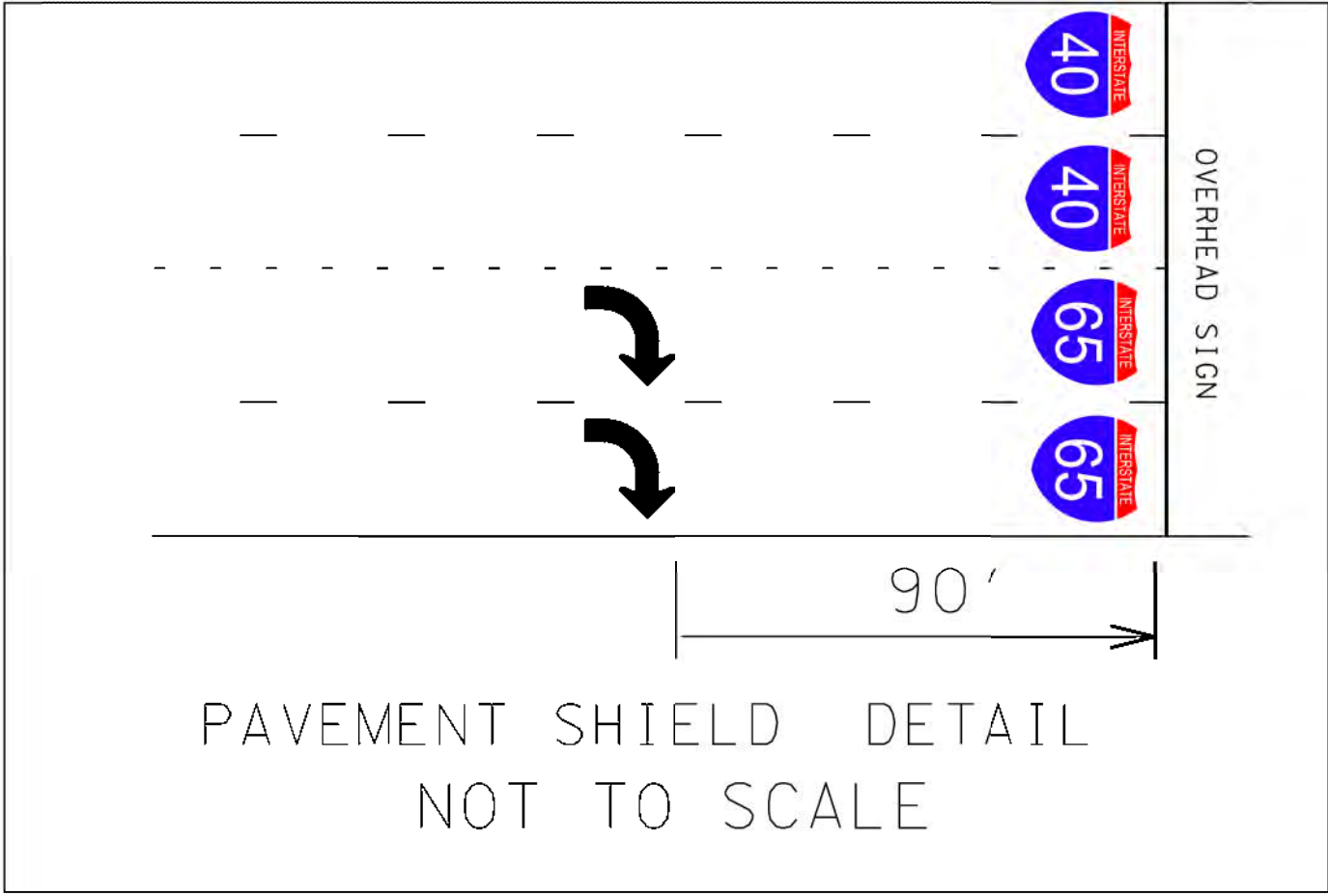
PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G6



INTERSTATE 40
18.02 - 18.22 LM
DAVIDSON COUNTY



REFERENCE STANDARD DRAWING
T-M-5, T-M-6

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FOR
REFERENCE
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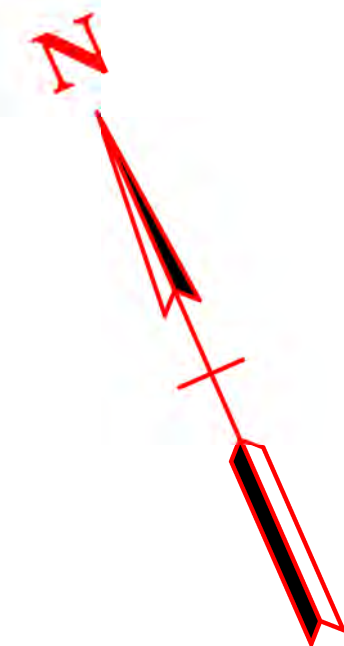
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

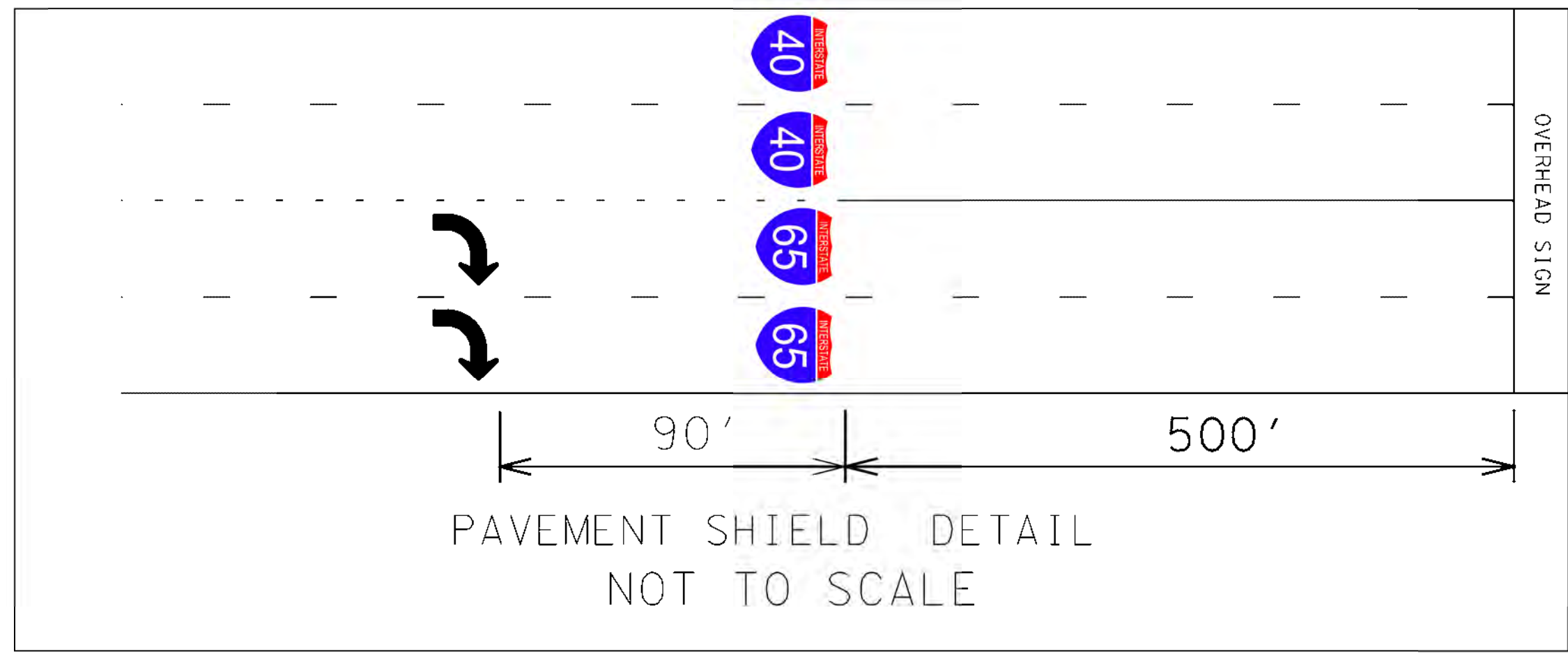
PAVEMENT
MARKING
IMPROVEMENTS

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MATCH LINE B



INTERSTATE 40
18.22 - 18.40 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-5, T-M-6

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	2G7

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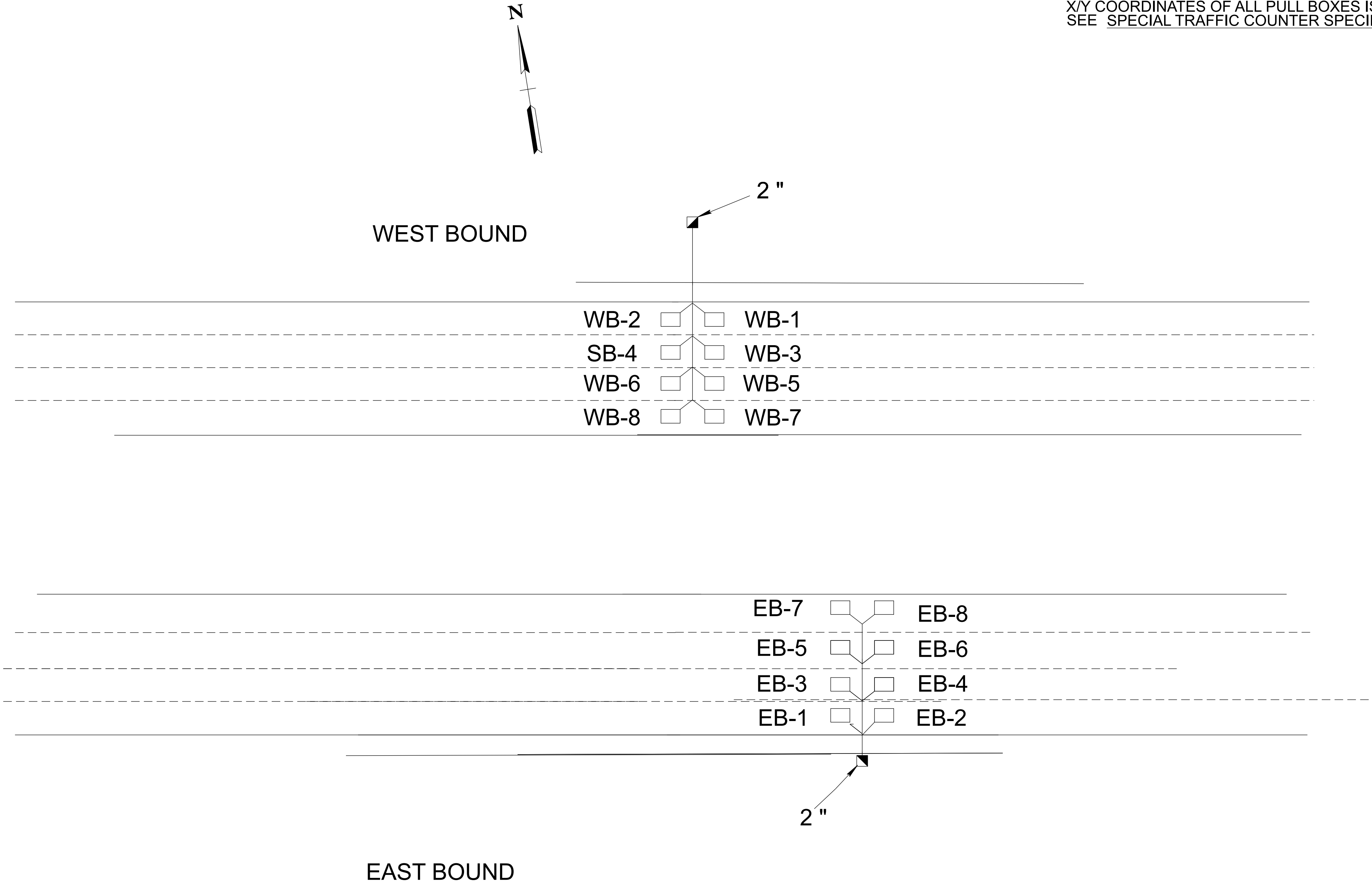
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

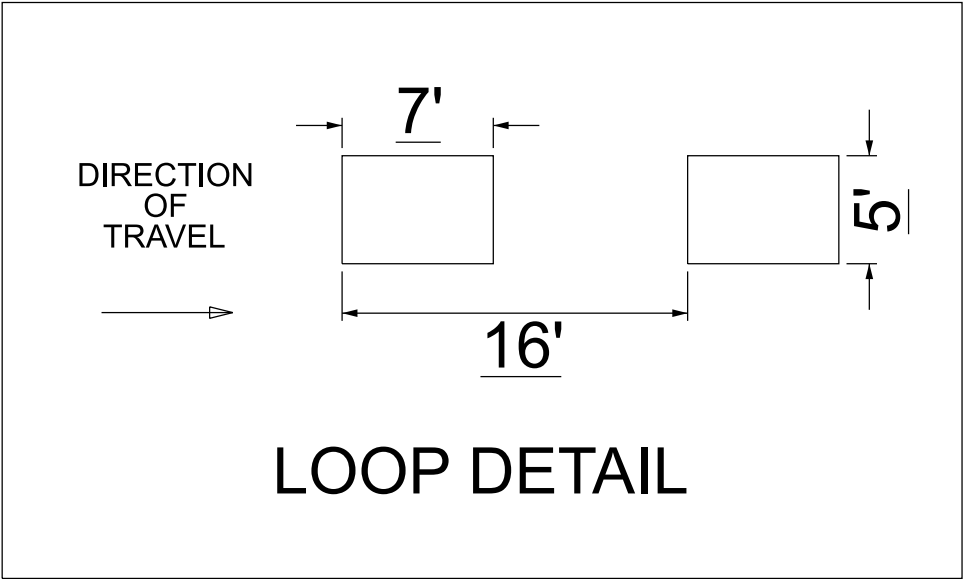
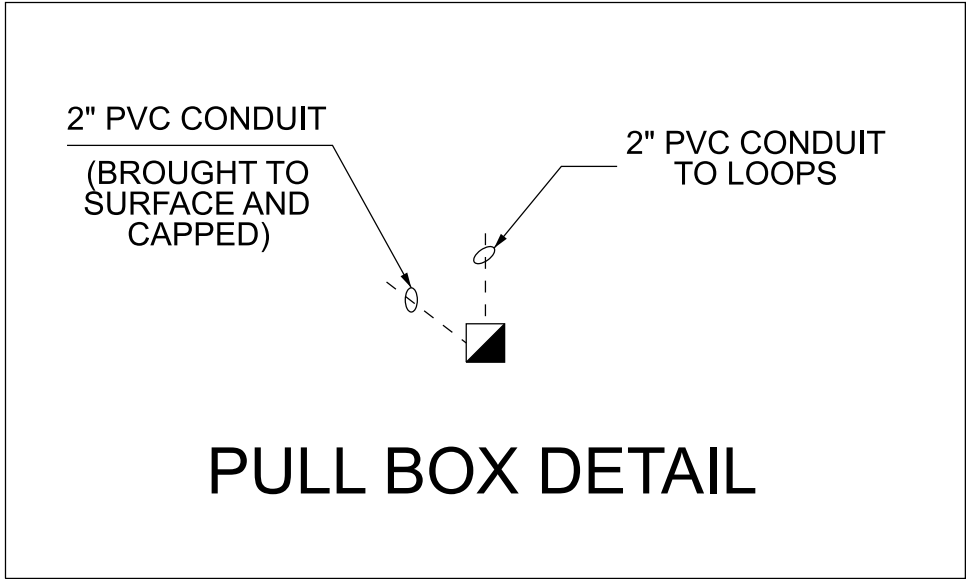
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	268

NOTE: NOTIFICATION OF COMPLETION AND THE
X/Y COORDINATES OF ALL PULL BOXES IS REQUIRED
SEE SPECIAL TRAFFIC COUNTER SPECIFICATIONS



I-24 - Davidson County
ATR-83
EAST BD LOG MILE 18.23
WEST BD LOG MILE 18.31

NOT TO SCALE



UTILITY NOTES

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

TELEPHONE & FIBER:

AT&T
116 South Cannon Ave
Murfreesboro, TN 37129
CONTACT: Lee Kornegay
OFFICE PHONE: 615 848 2082
CELL PHONE: ____ ____ ____
Email: Kk4096@att.com

CABLE:

COMCAST
660 Mainstream Dr
Nashville, TN 37228
CONTACT: Kevin Nipper
OFFICE PHONE: ____ ____ ____
CELL PHONE: 615 626 3325
Email: Kevinlee_Nipper@comcast.com
NAS-NashvilleConstructionBettermnts@comcast.com

WATER & SEWER:

METRO WATER & SEWER
1600 Second Ave North
Nashville, TN 37208
CONTACT: Paisley Marotta-Mathews
OFFICE PHONE: 615 862 4142
CELL PHONE: ____ ____ ____
Email: Paisley.marotta@nashville.gov

WATER & SEWER:

METRO WATER & SEWER
1600 Second Ave North
Nashville, TN 37208
CONTACT: Kecia Cain
OFFICE PHONE: 615 862 4801
CELL PHONE: ____ ____ ____
Email: Kecia.cain@nashville.gov

STREETLIGHTS & FIBER:

NASHVILLE DEPT. OF TRANSPORTATION (NDOT)
720 South Fifth Street
Nashville, TN 37206
CONTACT: Mike Hirtzer
OFFICE PHONE: 615 880 3261
CELL PHONE: ____ ____ ____
Email: michael.hirtzer@nashville.gov

ELECTRIC:

NASHVILLE ELECTRIC SERVICE
1214 Church Street, Room 363
Nashville, TN 37246
CONTACT: Jon Sipes
OFFICE PHONE: 615 747 3529
CELL PHONE: ____ ____ ____
Email: jsipes@nespower.com
utilityrelocations@nespower.com

ELECTRIC:

TENNESSEE VALLEY AUTHORITY (TVA)
1101 Market St MR-4G
Chattanooga, TN 37402
CONTACT: Stephen Williams
OFFICE PHONE: 662 255 6272
CELL PHONE: ____ ____ ____
Email: sewilliams@tva.gov

GAS:

PIEDMONT GAS
83 Century Boulevard
Nashville, TN 37214
CONTACT: Scott Hazzard
OFFICE PHONE: ____ ____ ____
CELL PHONE: 615 714 2389
Email: Scotty.Hazzard@duke-energy.com
NGBU.TN.relocations@duke-energy.com

GAS:

PIEDMONT GAS
83 Century Boulevard
Nashville, TN 37214
CONTACT: Nate Miller
OFFICE PHONE: 615 872 2457
CELL PHONE: ____ ____ ____
Email: nathaniel.miller2@duke-energy.com

FIBER OPTIC:

AT&T FIBER OPTIC CABLE
360 Gees Mill Business Pkwy
Conyers, GA 30013
CONTACT: Trina Ivey
OFFICE PHONE: ____ ____ ____
CELL PHONE: 678 641 5522
Email: ki2863@att.com

FIBER OPTIC:

CROWN CASTLE
6325 Ardrey Kell Rd, Suite 600
Charlotte, NC 28277
CONTACT: Joe Baluha
OFFICE PHONE: 336 562 7304
CELL PHONE: ____ ____ ____
Email: Joe.Baluha@crowncastle.com

FIBER OPTIC:

CROWN CASTLE
370 Mallory Station Rd, Suite 505
Franklin, TN 37067
CONTACT: Drew Parker
OFFICE PHONE: 630 480 5177
CELL PHONE: ____ ____ ____
Email: Drew.Parker@crowncastle.com

FIBER OPTIC:

GOOGLE NETWORK
1101 McGavock St, Suite 200
Nashville, TN 37203
CONTACT: Rick Bolton
OFFICE PHONE: 629 888 2258
CELL PHONE: ____ ____ ____
Email: boltonr@google.com

FIBER OPTIC:

GOOGLE NETWORK
1101 McGavock St, Suite 200
Nashville, TN 37203
CONTACT: Joshua Day
OFFICE PHONE: 615 962 4402
CELL PHONE: ____ ____ ____
Email: joshuaday@google.com

FIBER OPTIC:

LUMEN (FORMERLY CENTURYLINK & LEVEL 3 COMMUNICATIONS)
520 Whaley St
Columbia, SC 29201
CONTACT: John Boedeker
OFFICE PHONE: ____ ____ ____
CELL PHONE: 512 334 8351
Email: John.boedeker@lumen.com

FIBER OPTIC:

LUMEN (FORMERLY CENTURYLINK & LEVEL 3 COMMUNICATIONS)
2530 Perimeter Place Dr
Nashville, TN 37214
CONTACT: Jeffrey Cannon
OFFICE PHONE: 615 263 1128
CELL PHONE: ____ ____ ____
Email: jeffrey.cannon@lumen.com

FIBER OPTIC:

XO COMMUNICATIONS (VERIZON)
575 Hickory Hills Blvd
Whites Creek, TN 37189
CONTACT: Derek Dee
OFFICE PHONE: 615 777 7727
CELL PHONE: 615 207 1297
Email: Derek.r.dee@verizon.com

FIBER OPTIC:

XO COMMUNICATIONS (VERIZON)
575 Hickory Hills Blvd
Whites Creek, TN 37189
CONTACT: Michael Weaver
OFFICE PHONE: ____ ____ ____
CELL PHONE: 615 218 0656
Email: Michael.weaver@verizon.com

FIBER OPTIC:

ZAYO GROUP
4701 W Hillsborough Ave
Tampa, FL 33614
CONTACT: Jake Sansom
OFFICE PHONE: ____ ____ ____
CELL PHONE: 813 763 5999
Email: jake.sansom@zayo.com
zayo.relo.tennessee@zayo.com

FIBER OPTIC:

ZAYO GROUP
2030 Powers Ferry Rd SE, Suite 306
Atlanta, GA 30339
CONTACT: Jamye Debardeleben
OFFICE PHONE: ____ ____ ____
CELL PHONE: 678 755 7281
Email: jamye.debardeleben@zayo.com

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	3

SEALED BY



9/24/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-40-4(93)	T1

SEALED BY



9/24/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

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